

**File Ref.: SBCR 1/606/16**

## **LEGISLATIVE COUNCIL BRIEF**

Public Order Ordinance  
(Chapter 245)

### **CLOSED AREA (HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT AND HONG KONG LINK ROAD) ORDER**

### **HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT AND HONG KONG LINK ROAD CLOSED AREA (PERMISSION TO ENTER) NOTICE**

Immigration Ordinance  
(Chapter 115)

### **IMMIGRATION (PLACES OF DETENTION) (AMENDMENT) ORDER 2017**

Immigration Service Ordinance  
(Chapter 331)

### **IMMIGRATION SERVICE (DESIGNATED PLACES) (AMENDMENT) ORDER 2017**

## **INTRODUCTION**

To cater for the commissioning of the Hong Kong-Zhuhai-Macao Bridge (HZMB) Hong Kong Port (HKP) upon the opening of the Bridge, we need to enact technical subsidiary legislation relating to the operations of the HKP. This paper aims to brief Members on the relevant legislative work.

2. At the meeting of the Executive Council on 26 September 2017, the Council ADVISED and the Chief Executive ORDERED that the Closed Area (Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and

Hong Kong Link Road) Order (the Order), at **Annex A**, should be made under section 36(1) of the Public Order Ordinance (Chapter 245) (POO) to designate the HKP and the related roads in the vicinity, including the HZMB Hong Kong Link Road (HKLR) as a closed area.

3. To grant general permission to cross-boundary passengers and drivers for entering or leaving the closed area, the Commissioner of Police (CP) has made the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road Closed Area (Permission to Enter) Notice (the Notice), at **Annex B**, under section 38A of the POO.

4. The Secretary for Security has made the Immigration (Places of Detention) (Amendment) Order 2017, at **Annex C**, and the Immigration Service (Designated Places) (Amendment) Order 2017, at **Annex D** (the Amendment Orders), under section 35(1) of the Immigration Ordinance (Chapter 115) and section 13A(9) of the Immigration Service Ordinance (Chapter 331) respectively, to set up detention quarters at the HKP for use by the Immigration Department in its routine immigration control and law enforcement, as in some other control points.

## **JUSTIFICATIONS**

### **THE ORDER**

5. The HZMB is the first cross-boundary land link connecting Guangdong, Hong Kong and Macao. A new control point, namely the HKP, will be set up at the east of the Hong Kong International Airport to provide clearance facilities for cross-boundary passengers, vehicles and cargoes using the HZMB. The HKP will be connected to the HZMB Main Bridge by the HKLR, which is a 12-kilometre dual three-lane carriageway. A map (the location map) showing the locations of the HKP and HKLR is attached to the Order at **Annex A**.

6. According to the experience of the Police, land boundary control points are black spots for certain illegal activities, such as pick-pocketing, illegal immigration and smuggling. At present, the land boundary control points are administered by the closed area approach either under the POO or the Shenzhen Bay Port Hong Kong Port Area Ordinance (Chapter 591). We consider it necessary to designate the HKP and the related areas as a closed area following the practice for other land boundary control points. Otherwise, with its expected large

number of passengers and areas covered, the HKP would be subject to various law and order problems. It could become a hotbed of criminals engaging in illegal activities, and its operation would be seriously affected. The proposed closed area restriction shall take effect on a day to be appointed by the Secretary for Security, which will be the day on which the HZMB and HKP come into operation.

7. The proposed closed area under the Order covers the following parts –

- (a) **clearance and related areas in the HKP** – the customs, immigration and quarantine (CIQ) clearance for passengers, vehicles and cargoes departing from and arriving at Hong Kong via the HZMB will be conducted either at the Passenger Clearance Building (PCB), the Vehicle Clearance Plaza or the coach and shuttle bus kiosks at the HKP. To minimise the disruption to the CIQ operations, the proposed closed area should cover all these areas as well as other ‘post-clearance’ areas<sup>1</sup>. It however should not cover the facilities which, for the purpose of making the HKP a more user-friendly and convenient control point, are designed for public access without the need for a closed area permit, such as the private car pick-up/drop-off, public carparks, public transport interchange and publicly-accessible retail facilities. The areas in the HKP to be included in the proposed closed area are shown in Map 1 attached to the Order at **Annex A**;
- (b) **access roads to HKP** – to prevent the unintentional entry of vehicles into the HKP closed area, which may cause unnecessary traffic disruption to the area, the access roads leading to the HKP closed area

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<sup>1</sup> ‘Post-clearance’ areas refer to areas which are only accessible by departing passengers after undergoing departure clearance or arriving passengers before undergoing arrival clearance. For the HKP, the ‘post-clearance’ areas include the cross-boundary coach and cross-boundary shuttle bus pick-up and drop-off area located at the north of the PCB and the related road connections in the vicinity, etc.

should also be delineated as closed area. This is in line with the practice for other land boundary control points in Hong Kong. The relevant areas are also shown in Map 1 attached to the Order at **Annex A**; and

- (c) **the HKLR** – the entire HKLR which connects the HKP and the HZMB Main Bridge is located in the ‘post-clearance’ areas. For both security and immigration control purposes, the whole stretch of the HKLR should be delineated as closed area to enable efficient law enforcement. The relevant areas are shown in Maps 2-5 attached to the Order at **Annex A**.

8. Having regard to the specific circumstances of the HZMB control point, the designation of closed area would need to be three-dimensional, i.e. the closed area will need to be defined with reference to both horizontal and vertical boundaries. Such ‘three-dimensional’ designation approach was also adopted for the Shenzhen Bay Port Hong Kong Port Area and the Shenzhen Bay Bridge. It is necessary to adopt this approach for the HKP and related areas because there will be overlapping viaducts/roads in the HKP and some of the viaducts/roads in the closed area will be directly above/underneath viaducts/roads which should remain non-closed area. Also, there will be vessels sailing below and across the HKLR and hence the sea space underneath the HKLR must not be caught by the closed area.

9. The lower vertical boundary of the closed area varies from one area to another depending on the specific circumstances of each area, such as the existence of underground structures and overlapping viaducts/roads. As for the upper vertical boundaries, our intention is to cover the top of all structures in the proposed closed area but not the airspace in which aircraft may fly (bearing in mind the close proximity of the Hong Kong International Airport). Having regard to the height of the highest structure in the proposed closed area (around 70 to 80 metres above Hong Kong Principal Datum) and having consulted the

Civil Aviation Department, the upper vertical boundary of the entire closed area is proposed to be 80 metres above Hong Kong Principal Datum but subject to the height restriction set out in section 3(1) of the Hong Kong Airport (Control of Obstructions) (No.2) Order 1997 (Chapter 301D), which is the maximum height of building which can be built in the light of aviation safety concern. Since all structures allowed to be built in the area must be lower than the height restriction, the upper vertical boundary set in this way would be able to serve our policy intent as mentioned above. Apart from the written descriptions on the horizontal and vertical boundaries, a set of maps are also used to accurately describe the closed area to be designated.

### THE NOTICE

10. Following the practice of other land boundary control points, after the designation of the closed area, it would be necessary for the CP to make a notice under section 38A of the POO granting general permission to cross-boundary passengers and drivers for entering or leaving the closed area. Such permission aims at providing facilitation to cross-boundary passengers and drivers such that they can enter or leave the closed area without the need for a closed area permit. The effective date of the Notice will align with the above designation of closed area.

11. Section 1 of the Notice at **Annex B** provides that the commencement of the Notice is to be appointed by the Secretary for Security by notice published in the Gazette. Section 3 provides that a person of a category specified in column 2 of Part 2 of the Schedule is permitted to enter or leave the closed area at any time subject to the conditions applicable to the person of that category.

12. Under Part 2 of the Schedule, permission is granted to drivers of cross-boundary road vehicles, cross-boundary passengers on road vehicles as well as outbound passengers who enter the Pick-up Area or the PCB for the purpose of leaving Hong Kong by cross-boundary road vehicles but have not yet boarded the vehicles for entering or leaving the closed area subject to the conditions specified in column 3 of that Part.

### THE AMENDMENT ORDERS

13. Apart from the Order and the Notice, it is necessary to make

subsidiary legislation for setting up detention quarters at the HKP for use by the Immigration Department in its routine immigration control and law enforcement, as in some other control points like the Hong Kong International Airport, Lok Ma Chau Spur Line and Shenzhen Bay Hong Kong Port Area control points. The subsidiary legislation should also take effect when the HZMB and HKP come into operation.

14. Section 1 of the Amendment Orders at **Annex C** and **Annex D** provides that the Amendment Orders would come into operation on a day to be appointed by the Secretary for Security by notices published in the Gazette. Section 2 of the Immigration (Places of Detention) (Amendment) Order 2017 amends Schedule 3 of the Immigration (Places of Detention) Order (Chapter 115B) by adding the new detention quarters within the HKP. Likewise, Section 2 of the Immigration Service (Designated Places) (Amendment) Order 2017 so amends the Schedule to the Immigration Service (Designated Places) Order (Chapter 331B).

## **LEGISLATIVE TIMETABLE**

15. The legislative timetable for the above subsidiary legislation is as follows –

Publication in the Gazette	6 October 2017
Tabling at the Legislative Council (for negative vetting)	11 October 2017
Commencement	On a day to be appointed by the Secretary for Security by notice published in the Gazette, which will be the day on which the HZMB and HKP come into operation

## **IMPLICATIONS OF THE PROPOSALS**

16. The proposals are in conformity with the Basic Law, including the provisions concerning human rights. They do not affect the current binding effect of the POO, the Immigration Ordinance (Chapter 115) and the Immigration Service Ordinance (Chapter 331). They have no financial, civil service, economic, competition, productivity, environmental, sustainability, family or gender implications.

## **PUBLIC CONSULTATION**

17. We informed the Legislative Council Panel on Security on 27 July 2017 of the proposed legislative amendments through an information note. Members raised no queries or objection.

## **PUBLICITY**

18. A press release will be issued on 4 October 2017. A spokesperson will be available to answer media and public enquiries. Nearer the time of commencement of the Order, suitable measures will be put in place to inform users of the HKP (who can be passengers or drivers from Hong Kong and the Mainland) of the details of the closed area to ensure smooth flow of passengers and vehicles.

## **ENQUIRIES**

19. Enquiries on this brief can be addressed to Mr Parson Lam, Principal Assistant Secretary for Security (Tel: 2810 2506).

**Security Bureau**  
**October 2017**

## Closed Area (Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road) Order

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## Closed Area (Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road) Order

(Made by the Chief Executive under section 36(1) of the Public Order Ordinance (Cap. 245) after consultation with the Executive Council)

### 1. Commencement

This Order comes into operation on a day to be appointed by the Secretary for Security by notice published in the Gazette.

### 2. Interpretation

In this Order—

**Hong Kong Link Road** (香港連接路) means the Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road delineated and shown edged red on the map titled “LOCATION MAP OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT AND HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD”, numbered “ISM2670 (LOCATION MAP)” and contained in Schedule 1;

**Hong Kong Port** (香港口岸) means the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port delineated and shown edged dark blue on the map titled “LOCATION MAP OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT AND HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD”, numbered “ISM2670 (LOCATION MAP)” and contained in Schedule 1.

### 3. Declaration of closed area

- (1) The area specified in subsection (2) is declared to be a closed area.



- (2) The area referred to in subsection (1) comprises—
- (a) the following zones, which are located at the Hong Kong Port—
    - (i) the pink zone, to the extent of the areas that are located at the Hong Kong Port, described in section 2 of Schedule 2;
    - (ii) the yellow zone, to the extent of the areas that are located at the Hong Kong Port, described in section 3 of Schedule 2;
    - (iii) the green zone, to the extent of the areas that are located at the Hong Kong Port, described in section 4 of Schedule 2;
    - (iv) the blue zone described in section 5 of Schedule 2;
    - (v) the magenta zone described in section 6 of Schedule 2;
    - (vi) the dark violet zone described in section 7 of Schedule 2;
    - (vii) the grey zone described in section 8 of Schedule 2;
  - (b) the following zones, which are located at or adjacent to the Hong Kong Link Road (other than the section of the Road located within the Hong Kong Port)—
    - (i) the pink zone, to the extent of the areas that are located at the Hong Kong Link Road, described in section 2 of Schedule 2;
    - (ii) the yellow zone, to the extent of the area that is located at the Hong Kong Link Road, described in section 3 of Schedule 2;
    - (iii) the light green zone described in section 9 of Schedule 2;

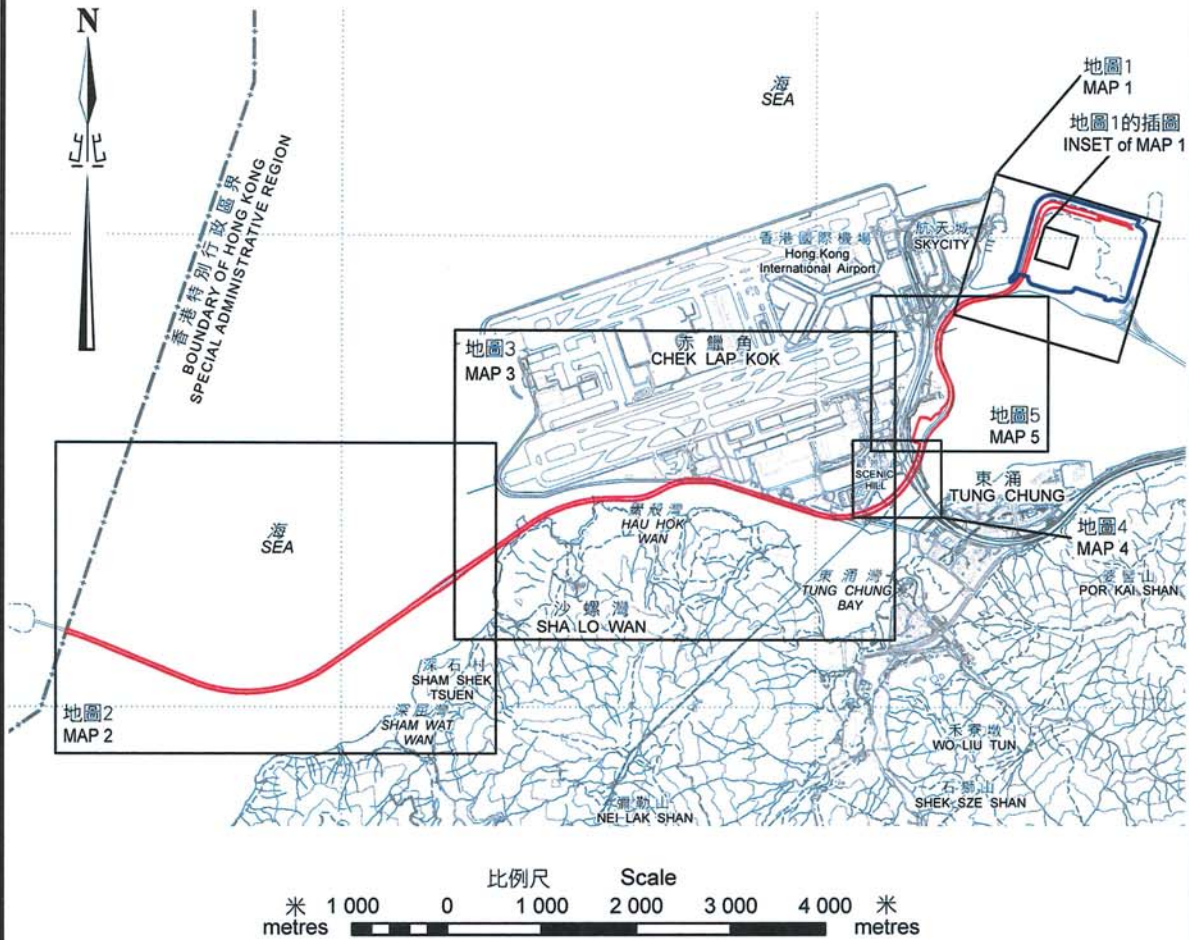
- (iv) the light blue zone described in section 10 of Schedule 2;
  - (v) the red zone described in section 11 of Schedule 2;
  - (vi) the orange zone described in section 12 of Schedule 2;
  - (vii) the dark blue zone described in section 13 of Schedule 2;
  - (viii) the violet zone described in section 14 of Schedule 2;
  - (ix) the beige zone described in section 15 of Schedule 2;
  - (x) the brown zone described in section 16 of Schedule 2; and
- (c) the following zones, which are adjacent to the Hong Kong Port and the Hong Kong Link Road—
- (i) the pink zone, to the extent of the areas that are located outside the Hong Kong Port and the Hong Kong Link Road, described in section 2 of Schedule 2;
  - (ii) the yellow zone, to the extent of the areas that are located outside the Hong Kong Port and the Hong Kong Link Road, described in section 3 of Schedule 2;
  - (iii) the green zone, to the extent of the areas that are located outside the Hong Kong Port and the Hong Kong Link Road, described in section 4 of Schedule 2.
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## **Schedule 1**

[s. 2]

## **Location Map**

港珠澳大橋香港口岸及港珠澳大橋香港連接路位置地圖  
 LOCATION MAP OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT  
 AND HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD



圖例 LEGEND

-  深藍色邊線 - 港珠澳大橋香港口岸  
 Edged dark blue - Hong Kong-Zhuhai-Macao Bridge Hong Kong Port
-  紅色邊線 - 港珠澳大橋香港連接路  
 Edged red - Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road

 地政總署 離島測量處  
 District Survey Office, Islands  
 Lands Department

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地圖編號 ISM2670 (位置地圖)  
 MAP No. (LOCATION MAP)

## Schedule 2

[s. 3]

### Zones Comprised in Closed Area

#### 1. Interpretation

In this Schedule—

*Inset of Map 1* (地圖 1 的插圖) means the map titled “INSET OF MAP 1 - PART OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT”, numbered “ISM2670 (INSET of MAP 1)” and contained in Schedule 3;

*Map 1* (地圖 1) means the map titled “MAP 1 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD, HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT AND ADJACENT AREAS”, numbered “ISM2670 (MAP 1)” and contained in Schedule 3;

*Map 4* (地圖 4) means the map titled “MAP 4 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD”, numbered “ISM2670 (MAP 4)” and contained in Schedule 3;

*Map 5* (地圖 5) means the map titled “MAP 5 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD”, numbered “ISM2670 (MAP 5)” and contained in Schedule 3.

#### 2. Pink zone

The pink zone comprises all of the areas each of which is enclosed by—

- (a) a horizontal boundary that is the outermost edge line of—
  - (i) an area located at the Hong Kong Port and shown coloured pink (including any area shown coloured pink cross-hatched black) on Map 1;
  - (ii) an area located at the Hong Kong Link Road and shown coloured pink on Map 1; or
  - (iii) an area located outside the Hong Kong Port and the Hong Kong Link Road, and shown coloured pink on Map 1;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at an elevation of –6 mPD (metres below Hong Kong Principal Datum).

#### 3. Yellow zone

The yellow zone comprises all of the areas each of which is enclosed by—

- (a) a horizontal boundary that is the outermost edge line of an area—
  - (i) located at the Hong Kong Port and shown coloured yellow cross-hatched black (indicated as a viaduct or section of a viaduct) on Map 1;
  - (ii) located at the Hong Kong Link Road and shown coloured yellow cross-hatched black (indicated as a viaduct or section of a viaduct) on Map 1; or
  - (iii) located outside the Hong Kong Port and the Hong Kong Link Road, and shown coloured yellow

- cross-hatched black (indicated as a viaduct or section of a viaduct) on Map 1;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at the bottom surface of the viaduct deck structure.

**4. Green zone**

The green zone comprises all of the areas each of which is enclosed by—

- (a) a horizontal boundary that is the outermost edge line of an area—
  - (i) located at the Hong Kong Port and shown coloured green cross-hatched black (indicated as the ground surface and at-grade road underneath viaduct) on Map 1; or
  - (ii) located outside the Hong Kong Port and the Hong Kong Link Road, and shown coloured green cross-hatched black (indicated as the ground surface and at-grade road underneath viaduct) on Map 1;
- (b) an upper vertical boundary set at the bottom surface of the viaduct deck structure; and
- (c) a lower vertical boundary set at an elevation of –6 mPD (metres below Hong Kong Principal Datum).

**5. Blue zone**

The blue zone is the area enclosed by—

- (a) a horizontal boundary that is the outermost edge line of the area shown coloured blue (indicated as a part of the Passenger Clearance Building) on the Inset of Map 1;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at an elevation of +12 mPD (metres above Hong Kong Principal Datum).

**6. Magenta zone**

- (1) Subject to subsection (2), the magenta zone is the area enclosed by—
  - (a) a horizontal boundary that is the outermost edge line of the area shown coloured magenta (indicated as a part of the Passenger Clearance Building) on the Inset of Map 1;
  - (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
  - (c) a lower vertical boundary set at an elevation of –6 mPD (metres below Hong Kong Principal Datum).
- (2) The magenta zone does not include the area enclosed by—
  - (a) a horizontal boundary that is the outermost edge line of the area shown coloured magenta (indicated as a part of the Passenger Clearance Building) on the Inset of Map 1;
  - (b) an upper vertical boundary set at the bottom surface of the floor slab structure of the first floor of the Passenger Clearance Building; and

- (c) a lower vertical boundary set at the top surface of the floor slab structure of the ground floor of the Passenger Clearance Building.

**7. Dark violet zone**

- (1) Subject to subsection (2), the dark violet zone comprises all of the areas each of which is enclosed by—
  - (a) a horizontal boundary that is the outermost edge line of an area shown coloured dark violet (indicated as a part of the Passenger Clearance Building) on the Inset of Map 1;
  - (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
  - (c) a lower vertical boundary set at an elevation of –6 mPD (metres below Hong Kong Principal Datum).
- (2) The dark violet zone does not include the areas each of which is enclosed by—
  - (a) a horizontal boundary that is the outermost edge line of an area shown coloured dark violet (indicated as a part of the Passenger Clearance Building) on the Inset of Map 1;
  - (b) an upper vertical boundary set at the bottom surface of the floor slab structure of the mezzanine floor of the Passenger Clearance Building; and
  - (c) a lower vertical boundary set at the top surface of the floor slab structure of the ground floor of the Passenger Clearance Building.

**8. Grey zone**

The grey zone is the area enclosed by—

- (a) a horizontal boundary that is the outermost edge line of the area shown coloured grey (indicated as a part of the main roof of the Passenger Clearance Building) on the Inset of Map 1;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at the bottom surface of the main roof structure of the Passenger Clearance Building.

**9. Light green zone**

The light green zone comprises all of the areas each of which is enclosed by—

- (a) a horizontal boundary that is the outermost edge line of an area shown coloured light green (indicated as a section of the Hong Kong Link Road with box culvert underneath) on Map 1 or Map 5;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at the top surface of the box culvert structure.

**10. Light blue zone**

- (1) The light blue zone comprises all of the areas each of which is enclosed by—

- (a) a horizontal boundary that is the outermost edge line of an area shown coloured light blue cross-hatched black (indicated as a section of a viaduct) on Map 2 or Map 3;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at the bottom surface of the viaduct deck structure.

(2) In subsection (1)—

**Map 2** (地圖 2) means the map titled “MAP 2 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD”, numbered “ISM2670 (MAP 2)” and contained in Schedule 3;

**Map 3** (地圖 3) means the map titled “MAP 3 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD”, numbered “ISM2670 (MAP 3)” and contained in Schedule 3.

#### 11. Red zone

The red zone is the area enclosed by—

- (a) a horizontal boundary that is the outermost edge line of the area shown coloured red on Map 4;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at an elevation of –6 mPD (metres below Hong Kong Principal Datum).

#### 12. Orange zone

The orange zone is the area enclosed by—

- (a) a horizontal boundary that is the outermost edge line of the area shown coloured orange (indicated as a part of the Scenic Hill Tunnel) on Map 4;
- (b) an upper vertical boundary set at the top surface of the Scenic Hill Tunnel; and
- (c) a lower vertical boundary set at the bottom surface of the Scenic Hill Tunnel.

#### 13. Dark blue zone

The dark blue zone is the area enclosed by—

- (a) a horizontal boundary that is the outermost edge line of the area shown coloured dark blue (indicated as a part of the Scenic Hill Tunnel) on Map 5;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at the bottom surface of the Scenic Hill Tunnel.

#### 14. Violet zone

The violet zone is the area enclosed by—

- (a) a horizontal boundary that is the outermost edge line of the area shown coloured violet (indicated as a ramp of the Scenic Hill Tunnel) on Map 5;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at the bottom surface of the ramp structure of the Scenic Hill Tunnel.

**15. Beige zone**

The beige zone comprises all of the areas each of which is enclosed by—

- (a) a horizontal boundary that is the outermost edge line of an area shown coloured beige (indicated as a section of the Hong Kong Link Road) on Map 5;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at an elevation of -6 mPD (metres below Hong Kong Principal Datum).

**16. Brown zone**

The brown zone is the area enclosed by—

- (a) a horizontal boundary that is the outermost edge line of the area shown coloured brown (indicated as a section of the Hong Kong Link Road with maintenance subway underneath) on Map 5;
- (b) an upper vertical boundary set, subject to section 17 of this Schedule, at an elevation of +80 mPD (metres above Hong Kong Principal Datum); and
- (c) a lower vertical boundary set at the top surface of the maintenance subway structure.

**17. Upper vertical boundary set at restricted height in particular cases**

- (1) If an upper vertical boundary mentioned in a relevant section in relation to an area is higher than the restricted height applicable to the area or part of the area, the upper vertical

boundary of the area or part is set at the restricted height applicable to the area or part (as the case requires).

- (2) In subsection (1)—

*relevant section* (有關條文) means section 2, 3, 5, 6(1), 7(1), 8, 9, 10(1), 11, 13, 14, 15 or 16 of this Schedule;

*restricted height* (限制高度), in relation to an area or a part of an area, means the height above the principal datum—

- (a) referred to in section 3(1) of the Hong Kong Airport (Control of Obstructions) (No. 2) Order 1997 (Cap. 301 sub. leg. D); and
- (b) applicable to the area or part under that Order.

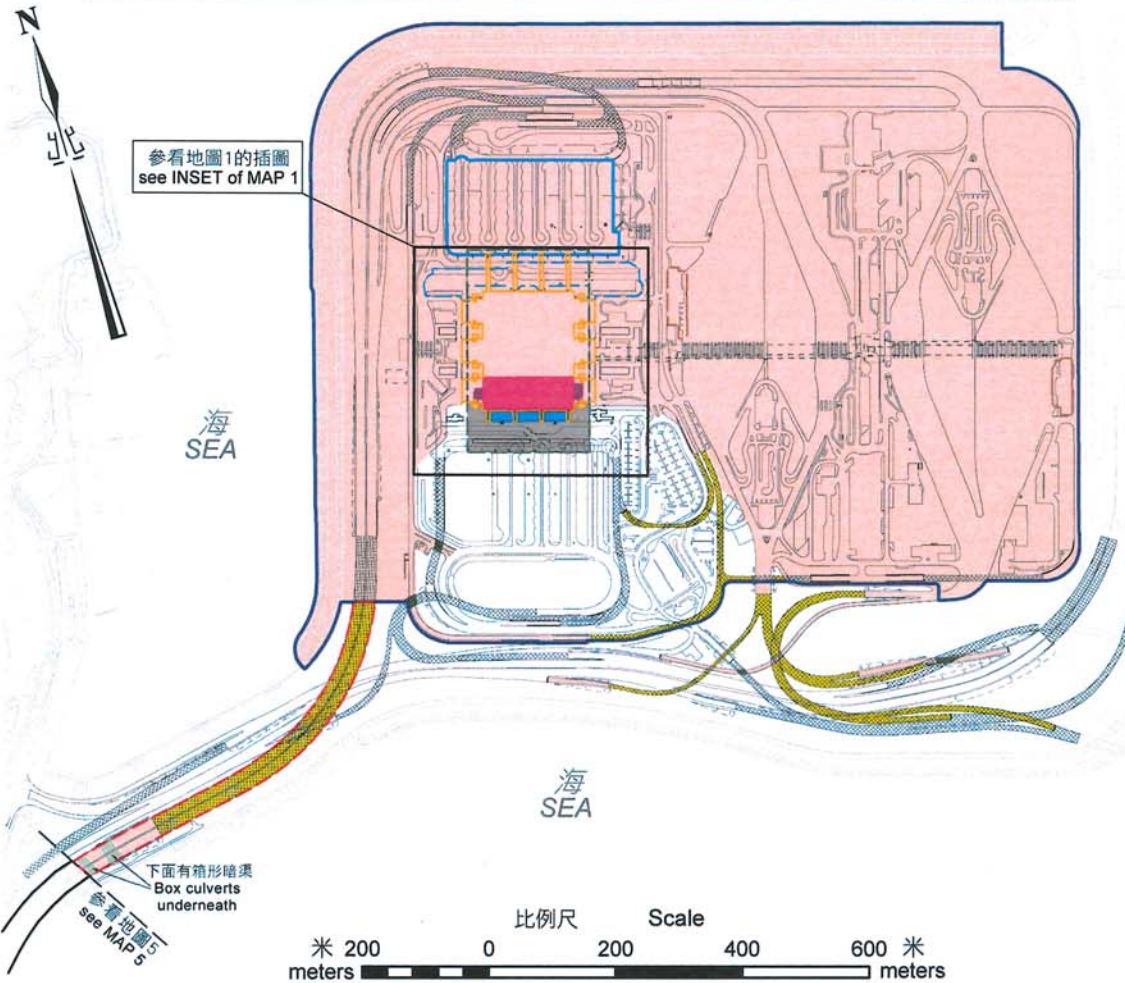


## **Schedule 3**












[Sch. 2]

## **Maps**

地圖 1 - 港珠澳大橋香港連接路路段、港珠澳大橋香港口岸及毗鄰地區  
 MAP 1 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG LINK ROAD,  
 HONG KONG-ZHUHAI-MACAO BRIDGE HONG KONG PORT AND ADJACENT AREAS



圖例 LEGEND

- |   |  |   |   |   |  |
|---|--|---|---|---|--|
|  | 縵黑交叉線 - 高架道路<br>Cross-hatched black - viaduct  |  | 棕色邊線 - 出境私家車旅客檢查站<br>Edged brown -<br>Departure Private Car Passenger Clearance Annex                     |  | 棕色虛邊線 - 入境私家車旅客檢查站<br>Edged pecked brown -<br>Arrival Private Car Passenger Clearance Annex                        |
|  | 粉紅色或粉紅色縵黑交叉線<br>Pink or pink cross-hatched black   |  | 藍色邊線 - 跨境巴士及跨境穿梭巴士上客區<br>Edged blue - cross-boundary coach and<br>cross-boundary shuttle bus pick-up area |  | 藍色虛邊線 - 跨境巴士及跨境穿梭巴士落客區<br>Edged pecked blue - cross-boundary coach and<br>cross-boundary shuttle bus drop-off area |
|  | 黃色縵黑交叉線 - 高架道路或高架道路路段<br>Yellow cross-hatched black - viaduct or section of a viaduct  |  | 深藍色邊線 - 港珠澳大橋香港口岸<br>Edged dark blue - Hong Kong-Zhuhai-Macao Bridge<br>Hong Kong Port                    |  | 紅色虛邊線 - 港珠澳大橋香港連接路路段<br>Edged pecked red - section of Hong<br>Kong-Zhuhai-Macao Bridge Hong Kong Link Road         |
|  | 綠色縵黑交叉線 - 高架道路下面的地面及地面道路<br>Green cross-hatched black - ground surface and at-grade road underneath viaduct                          |   |   |   |  |
|  | 淺綠色 - 下面有箱形暗渠的港珠澳大橋香港連接路路段<br>Light green - section of Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road with box culvert underneath |   |   |   |  |

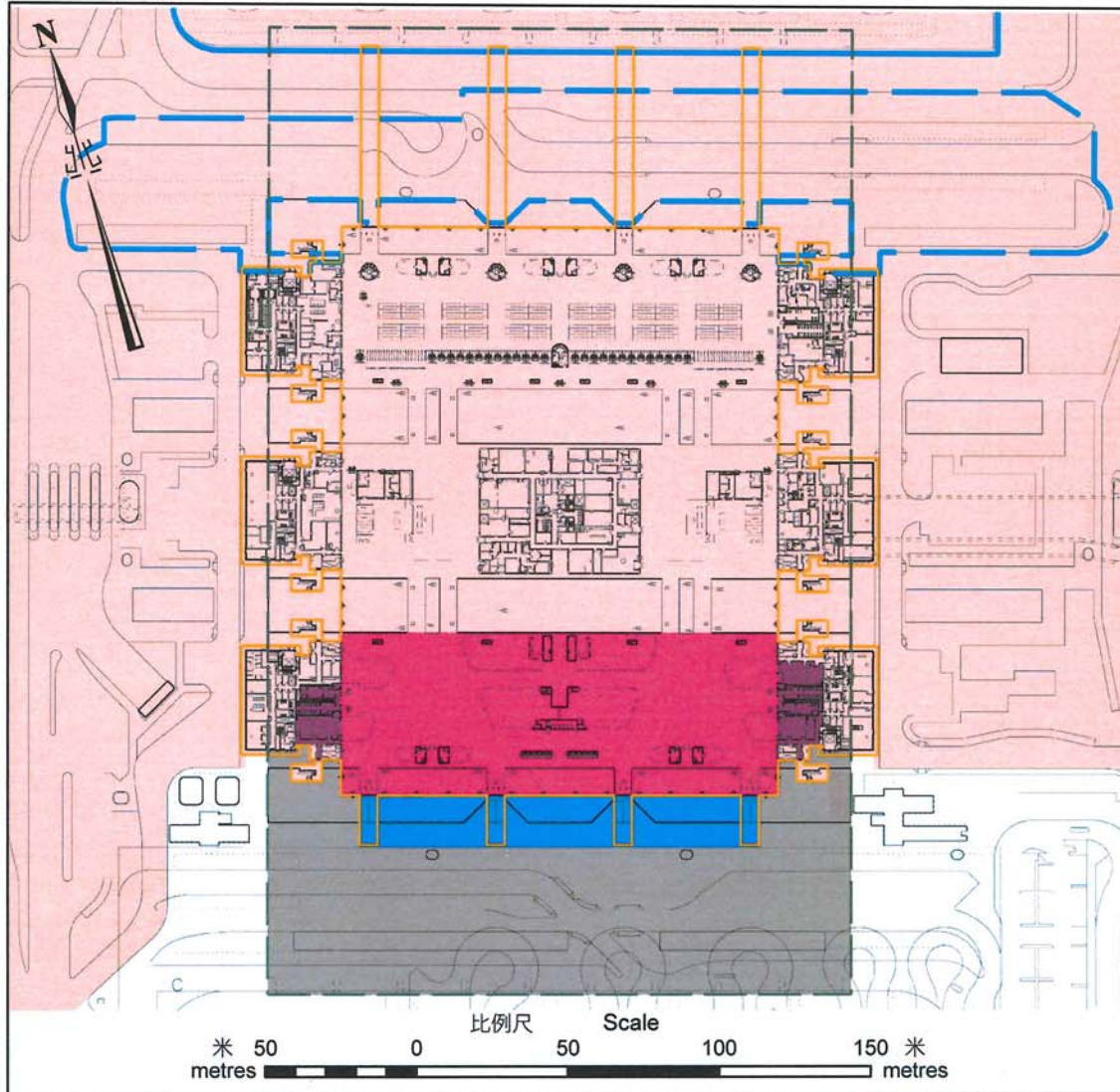
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地圖編號 ISM2670 (地圖1)  
 MAP No. (MAP 1)

地圖1的插圖 - 港珠澳大橋香港口岸部分  
INSET OF MAP 1 - PART OF HONG KONG-ZHUHAI-MACAO BRIDGE  
HONG KONG PORT



圖例 LEGEND

- |  |  |
|--|--|
| <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: pink; border: 1px solid black; margin-right: 5px;"></span> 粉紅色<br/>Pink</li> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: magenta; border: 1px solid black; margin-right: 5px;"></span> 品紅色 - 旅檢大樓部分<br/>Magenta - part of the Passenger Clearance Building</li> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: darkviolet; border: 1px solid black; margin-right: 5px;"></span> 深紫色 - 旅檢大樓部分<br/>Dark violet - part of the Passenger Clearance Building</li> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: blue; border: 1px solid black; margin-right: 5px;"></span> 藍色 - 旅檢大樓部分<br/>Blue - part of the Passenger Clearance Building</li> <li><span style="display: inline-block; width: 20px; height: 10px; background-color: grey; border: 1px solid black; margin-right: 5px;"></span> 灰色 - 旅檢大樓樓頂部分<br/>Grey - part of the main roof of the Passenger Clearance Building</li> </ul> | <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 20px; height: 10px; border: 1px dashed green; margin-right: 5px;"></span> 綠色虛邊線 - 旅檢大樓樓頂<br/>Edged pecked green - main roof of the Passenger Clearance Building</li> <li><span style="display: inline-block; width: 20px; height: 10px; border: 1px solid orange; margin-right: 5px;"></span> 橙色邊線 - 旅檢大樓<br/>Edged orange - Passenger Clearance Building</li> <li><span style="display: inline-block; width: 20px; height: 10px; border: 1px solid blue; margin-right: 5px;"></span> 藍色邊線 - 跨境巴士及<br/>跨境穿梭巴士上客區<br/>Edged blue - cross-boundary coach<br/>and cross-boundary shuttle bus pick-up area</li> <li><span style="display: inline-block; width: 20px; height: 10px; border: 1px dashed blue; margin-right: 5px;"></span> 藍色虛邊線 - 跨境巴士及<br/>跨境穿梭巴士落客區<br/>Edged pecked blue - cross-boundary coach<br/>and cross-boundary shuttle bus drop-off area</li> </ul> |
|--|--|



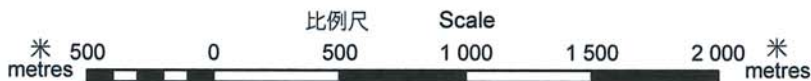
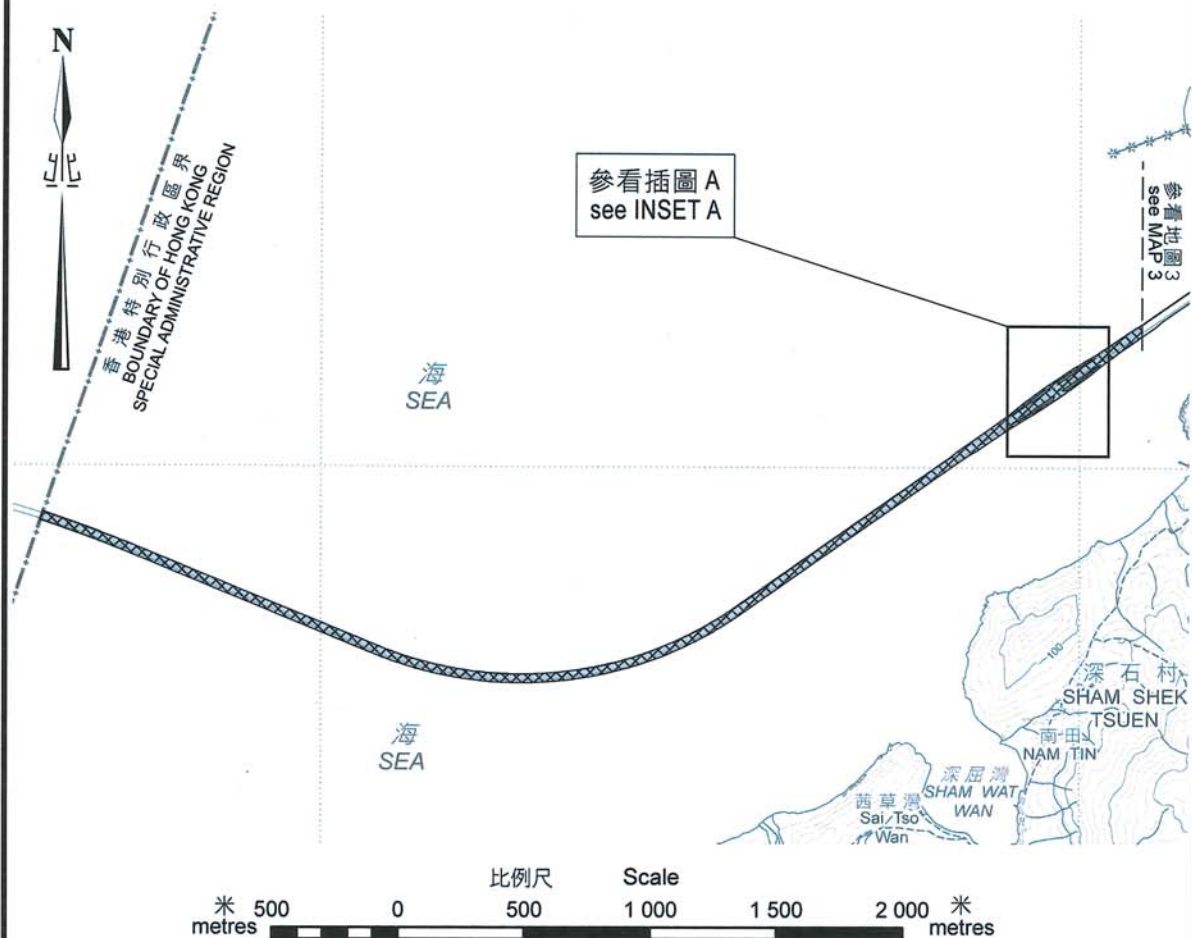
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地圖編號 ISM2670 (地圖1的插圖)  
MAP No. (INSET of MAP 1)

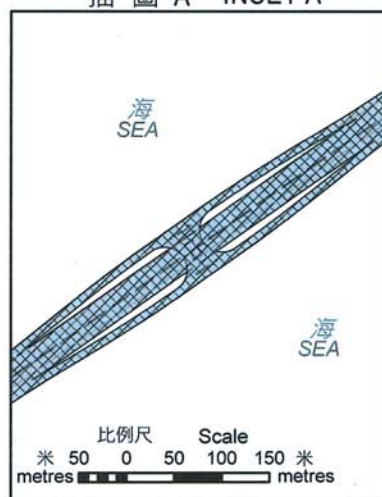
地圖 2 - 港珠澳大橋香港連接路路段  
 MAP 2 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE  
 HONG KONG LINK ROAD



圖例 LEGEND

 淺藍色綴黑交叉線 - 高架道路路段  
 Light blue cross-hatched black - section of a viaduct

插圖 A INSET A



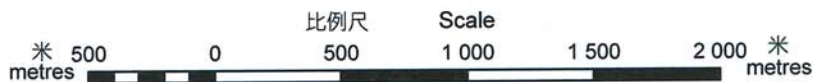
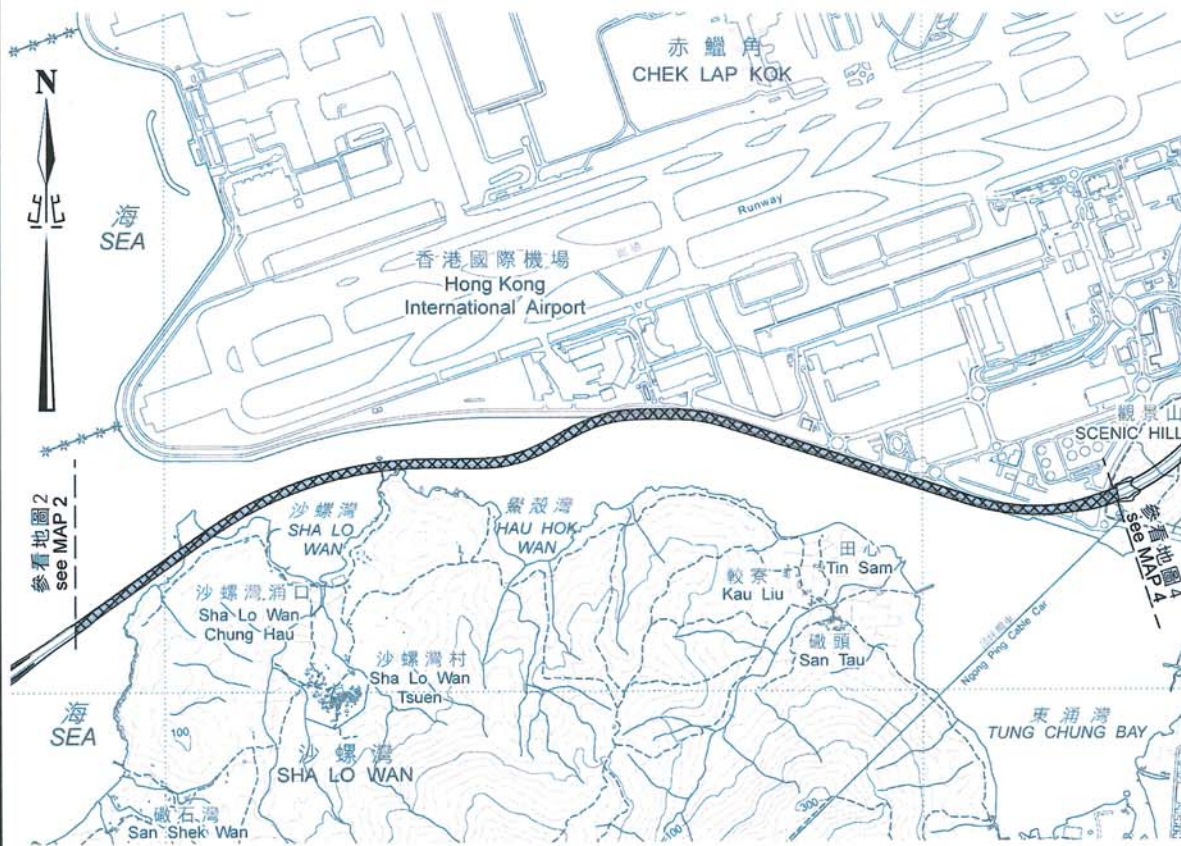
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地圖編號 MAP No. ISM2670 (地圖2) (MAP 2)

地圖3 - 港珠澳大橋香港連接路路段  
 MAP 3 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE  
 HONG KONG LINK ROAD



圖例 LEGEND

-  淺藍色綴黑交叉線 - 高架道路路段  
 Light blue cross-hatched black - section of a viaduct

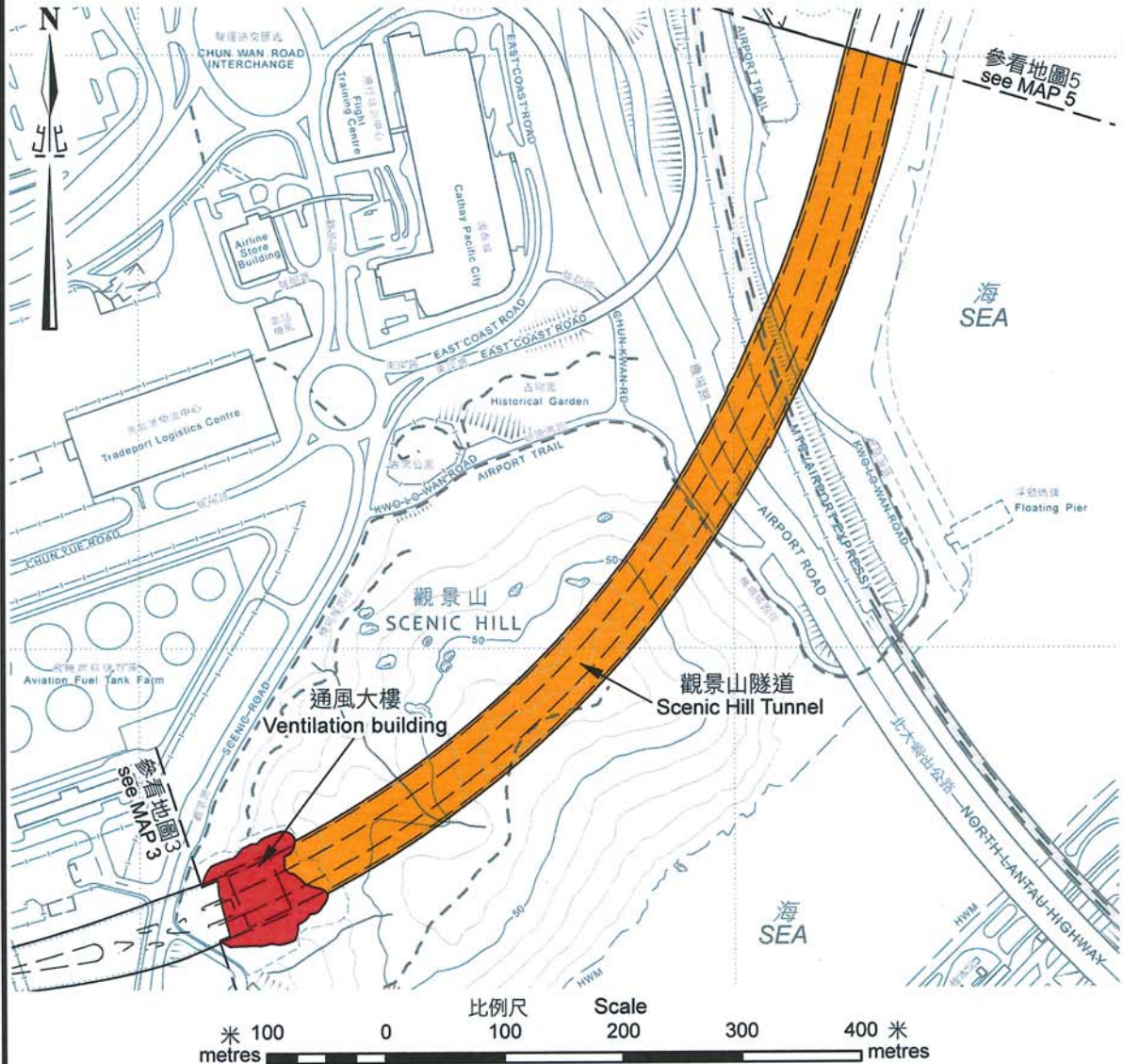
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地圖編號 ISM2670 (地圖3)  
 MAP No. ISM2670 (MAP 3)

地圖4 - 港珠澳大橋香港連接路路段  
 MAP 4 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE  
 HONG KONG LINK ROAD



圖例 LEGEND

- 紅色  
Red
- 橙色 - 觀景山隧道部分  
Orange - part of the Scenic Hill Tunnel

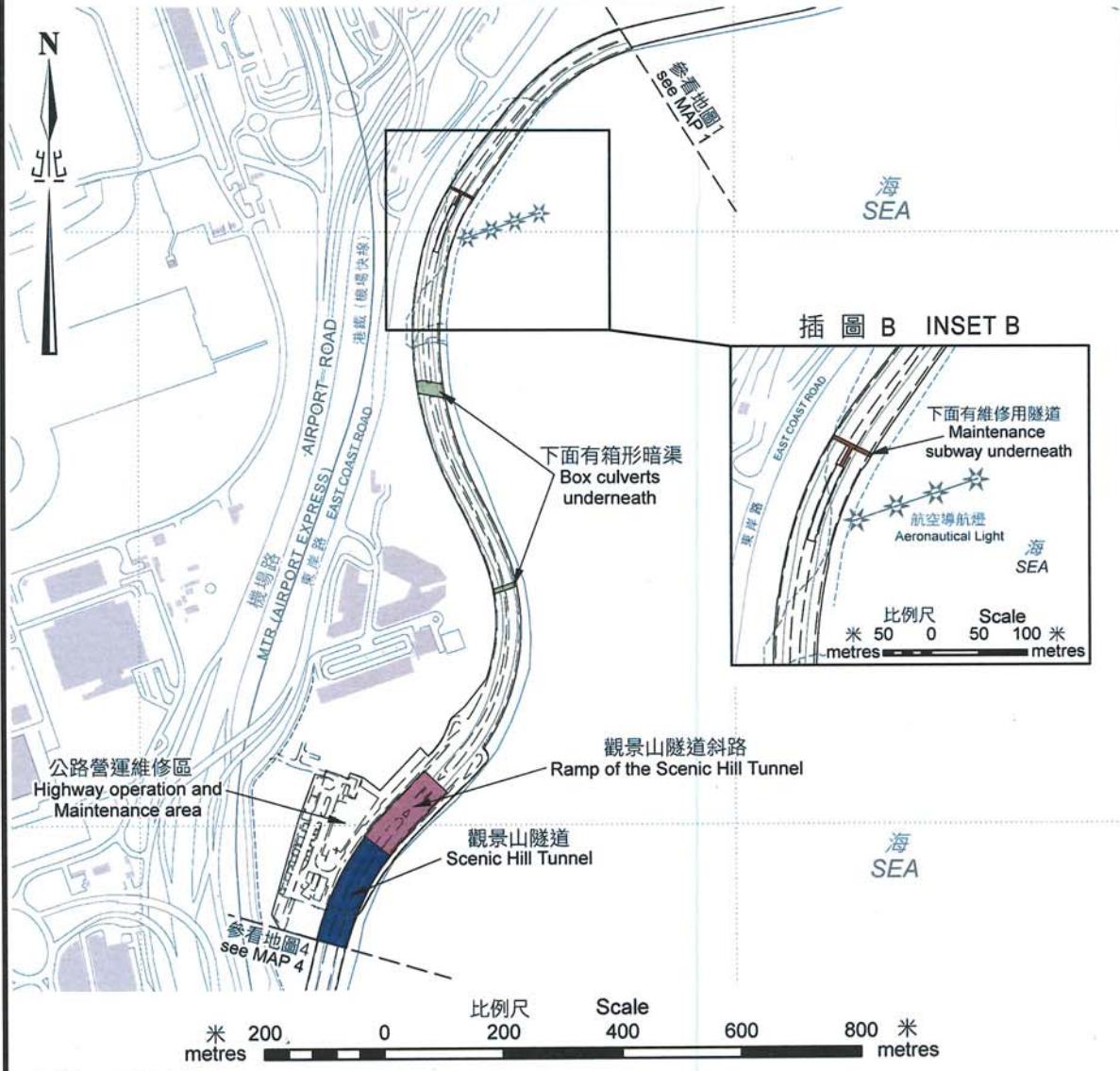
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地圖編號 ISM2670 (地圖4)  
 MAP No. (MAP 4)

地圖5 - 港珠澳大橋香港連接路路段  
MAP 5 - SECTION OF HONG KONG-ZHUHAI-MACAO BRIDGE  
HONG KONG LINK ROAD



圖例 LEGEND

- 深藍色 - 觀景山隧道部分  
Dark blue - part of the Scenic Hill Tunnel
- 紫色 - 觀景山隧道斜路  
Violet - ramp of the Scenic Hill Tunnel
- 米色 - 港珠澳大橋香港連接路路段  
Beige - section of Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road
- 淺綠色 - 下面有箱形暗渠的港珠澳大橋香港連接路路段  
Light green - section of Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road with box culvert underneath
- 棕色 - 下面有維修用隧道的港珠澳大橋香港連接路路段  
Brown - section of Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road with maintenance subway underneath

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地圖編號 MAP No. ISM2670 (地圖5) (MAP 5)



Chief Executive

27.9. 2017

### Explanatory Note

The purpose of this Order is to declare the area comprising the following areas to be a closed area under the Public Order Ordinance (Cap. 245)—

- (a) certain areas located at or adjacent to the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port, including the Passenger Clearance Building (other than certain parts at the ground floor of the Building);
- (b) certain areas located at or adjacent to the Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road.



## Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road Closed Area (Permission to Enter) Notice

(Made by the Commissioner of Police under section 38A of the Public  
Order Ordinance (Cap. 245))

### 1. Commencement

This Notice comes into operation on a day to be appointed by the  
Secretary for Security by notice published in the Gazette.

### 2. Interpretation

In this Notice—

**Closed Area** (禁區) means the area declared to be a closed area  
under section 3 of the Closed Area Order;

**Closed Area Order** (《禁區令》) means the Closed Area (Hong  
Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong  
Link Road) Order.

### 3. Permission to enter or leave

Without affecting any other Ordinance, a person of a category  
specified in column 2 of Part 2 of the Schedule is permitted to enter  
or leave the Closed Area at any time subject to all of the conditions  
specified in column 3 of that Part that are applicable to the person  
of that category.

## Schedule

[s. 3]

### Permission to Enter or Leave

#### Part 1

#### Interpretation

##### 1. Interpretation

In this Schedule—

**Arrival Area** (入境地區) means the area comprising the  
following—

- (a) the Drop-off Area;
- (b) the Passenger Clearance Building;

**Arrival Private Car Annex** (入境私家車站) means the Arrival  
Private Car Passenger Clearance Annex located at the Hong  
Kong Port;

**cross-boundary** (跨境) means travelling to or from the Mainland of  
China or Macao;

**Departure Area** (出境地區) means the area comprising the  
following—

- (a) the Pick-up Area;
- (b) the Passenger Clearance Building;

**Departure Private Car Annex** (出境私家車站) means the  
Departure Private Car Passenger Clearance Annex located at  
the Hong Kong Port;

**Drop-off Area** (落客區) means the cross-boundary coach and cross-boundary shuttle bus drop-off area located at the Hong Kong Port;

**Hong Kong Link Road** (香港連接路) has the meaning given by section 2 of the Closed Area Order;

**Hong Kong Port** (香港口岸) has the meaning given by section 2 of the Closed Area Order;

**Passenger Clearance Building** (旅檢大樓) means the part of the Passenger Clearance Building located at the Hong Kong Port that is part of the Closed Area;

**Pick-up Area** (上客區) means the cross-boundary coach and cross-boundary shuttle bus pick-up area located at the Hong Kong Port.

## Part 2

### Permission to Enter or Leave

Column 1	Column 2	Column 3
Item	Persons permitted to enter or leave	Conditions
1.	The driver of a road vehicle to the Mainland of China or Macao, leaving Hong Kong via the Hong Kong Port and the Hong Kong Link Road	<p>(a) The driver must drive the vehicle directly from the boundary of Hong Kong at the Hong Kong Link Road.</p> <p>(b) Subject to paragraphs (c) and (d), the driver must not leave the immediate</p>

Column 1	Column 2	Column 3
Item	Persons permitted to enter or leave	Conditions
		vicinity of the vehicle.
		(c) For a road vehicle using the Pick-up Area, the driver—
		(i) may leave the immediate vicinity of the vehicle but must not stay in any place other than the Departure Area; and
		(ii) may stay in the Departure Area only for the purpose of using the vehicle to pick up cross-boundary passengers.
		(d) For a road vehicle using the Departure Private Car Annex, the driver may leave the immediate vicinity of the vehicle but must not stay in any place other than the Departure Private Car Annex.
		(e) The driver must leave the

Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road  
Closed Area (Permission to Enter) Notice

Schedule—Part 2

5

Column 1	Column 2	Column 3
Item	Persons permitted to enter or leave	Conditions
		Closed Area without undue delay.
2.	The driver of a road vehicle from the Mainland of China or Macao, entering Hong Kong via the Hong Kong Link Road	<p>(a) The driver must drive the vehicle directly from the boundary of Hong Kong at the Hong Kong Link Road to the Hong Kong Port.</p> <p>(b) Subject to paragraphs (c) and (d), the driver must not leave the immediate vicinity of the vehicle.</p> <p>(c) For a road vehicle using the Drop-off Area, the driver—</p> <p>(i) may leave the immediate vicinity of the vehicle but must not stay in any place other than the Arrival Area; and</p> <p>(ii) may stay in the Arrival Area only for the purpose of using the vehicle to drop off cross-</p>

Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road  
Closed Area (Permission to Enter) Notice

Schedule—Part 2

6

Column 1	Column 2	Column 3
Item	Persons permitted to enter or leave	Conditions
		boundary passengers.
		(d) For a road vehicle using the Arrival Private Car Annex, the driver may leave the immediate vicinity of the vehicle but must not stay in any place other than the Arrival Private Car Annex.
		(e) The driver must leave the Closed Area without undue delay.
3.	A cross-boundary passenger on a road vehicle to the Mainland of China or Macao, leaving Hong Kong via the Hong Kong Port and the Hong Kong Link Road	<p>(a) Subject to paragraphs (b) and (c), the passenger must not leave the immediate vicinity of the vehicle.</p> <p>(b) For a road vehicle using the Pick-up Area, the passenger—</p> <p>(i) may leave the immediate vicinity of the vehicle but must not stay in any</p>

Column 1	Column 2	Column 3
Item	Persons permitted to enter or leave	Conditions
		place other than the Departure Area; and
		(ii) may stay in the Departure Area only for the purpose of using the vehicle to leave Hong Kong.
		(c) For a road vehicle using the Departure Private Car Annex, the passenger may leave the immediate vicinity of the vehicle but must not stay in any place other than the Departure Private Car Annex.
		(d) The passenger must leave the Closed Area without undue delay.
4.	A cross-boundary passenger on a road vehicle from the Mainland of China or Macao, entering Hong Kong via the Hong Kong Link Road	(a) Subject to paragraphs (b) and (c), the passenger must not leave the immediate vicinity of the vehicle. (b) For a road vehicle using

Column 1	Column 2	Column 3
Item	Persons permitted to enter or leave	Conditions
		the Drop-off Area, the passenger may leave the immediate vicinity of the vehicle but must not stay in any place other than the Arrival Area.
		(c) For a road vehicle using the Arrival Private Car Annex, the passenger may leave the immediate vicinity of the vehicle but must not stay in any place other than the Arrival Private Car Annex.
		(d) The passenger must leave the Closed Area without undue delay.
5.	A person who—	The person—
	(a) enters the Departure Area for the sole purpose of leaving Hong Kong by a road vehicle that provides cross-	(a) must not stay in any place other than the Departure Area;
		(b) may stay in the Departure Area only for the purpose of boarding the

Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road  
Closed Area (Permission to Enter) Notice

Schedule—Part 2

9

Column 1	Column 2	Column 3
Item	Persons permitted to enter or leave	Conditions
	boundary services; and	vehicle to leave Hong Kong; and
	(b) has not yet boarded the vehicle	(c) must board the vehicle without undue delay.

  
Acting Commissioner of Police

28<sup>th</sup> September 2017

Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road  
Closed Area (Permission to Enter) Notice

Explanatory Note  
Paragraph 1

10

**Explanatory Note**

The object of this Notice is to grant permission to persons leaving Hong Kong via the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and the Hong Kong-Zhuhai-Macao Bridge Hong Kong Link Road (*Hong Kong Link Road*) or entering Hong Kong via the Hong Kong Link Road to enter or leave the area declared to be a closed area under section 3 of the Closed Area (Hong Kong-Zhuhai-Macao Bridge Hong Kong Port and Hong Kong Link Road) Order.

## Immigration (Places of Detention) (Amendment) Order 2017

(Made by the Secretary for Security under section 35(1) of the Immigration Ordinance (Cap. 115))

### 1. Commencement

This Order comes into operation on a day to be appointed by the Secretary for Security by notice published in the Gazette.

### 2. Immigration (Places of Detention) Order amended

The Immigration (Places of Detention) Order (Cap. 115 sub. leg. B) is amended as set out in section 3.

### 3. Schedule 3 amended

Schedule 3—

#### Add

“30. The area within the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port that is set aside as detention quarters for use by the Immigration Department.”.



Secretary for Security

29th September 2017

### Explanatory Note

An area within the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port is to be set aside as detention quarters for use by the Immigration Department.

2. This Order amends the Immigration (Places of Detention) Order (Cap. 115 sub. leg. B) so that the area can be used as a place of detention under the Immigration Ordinance (Cap. 115).

## Immigration Service (Designated Places) (Amendment) Order 2017

(Made by the Secretary for Security under section 13A(9) of the  
Immigration Service Ordinance (Cap. 331))

### 1. Commencement

This Order comes into operation on a day to be appointed by the Secretary for Security by notice published in the Gazette.

### 2. Immigration Service (Designated Places) Order amended

The Immigration Service (Designated Places) Order (Cap. 331 sub. leg. B) is amended as set out in section 3.

### 3. Schedule amended

The Schedule—

#### Add

“16. The area within the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port that is set aside as detention quarters for use by the Immigration Department.”



Secretary for Security

29th September 2017

### Explanatory Note

An area within the Hong Kong-Zhuhai-Macao Bridge Hong Kong Port is to be set aside as detention quarters for use by the Immigration Department.

2. This Order amends the Immigration Service (Designated Places) Order (Cap. 331 sub. leg. B) so that the area becomes a designated place for the purposes of section 13A of the Immigration Service Ordinance (Cap. 331).